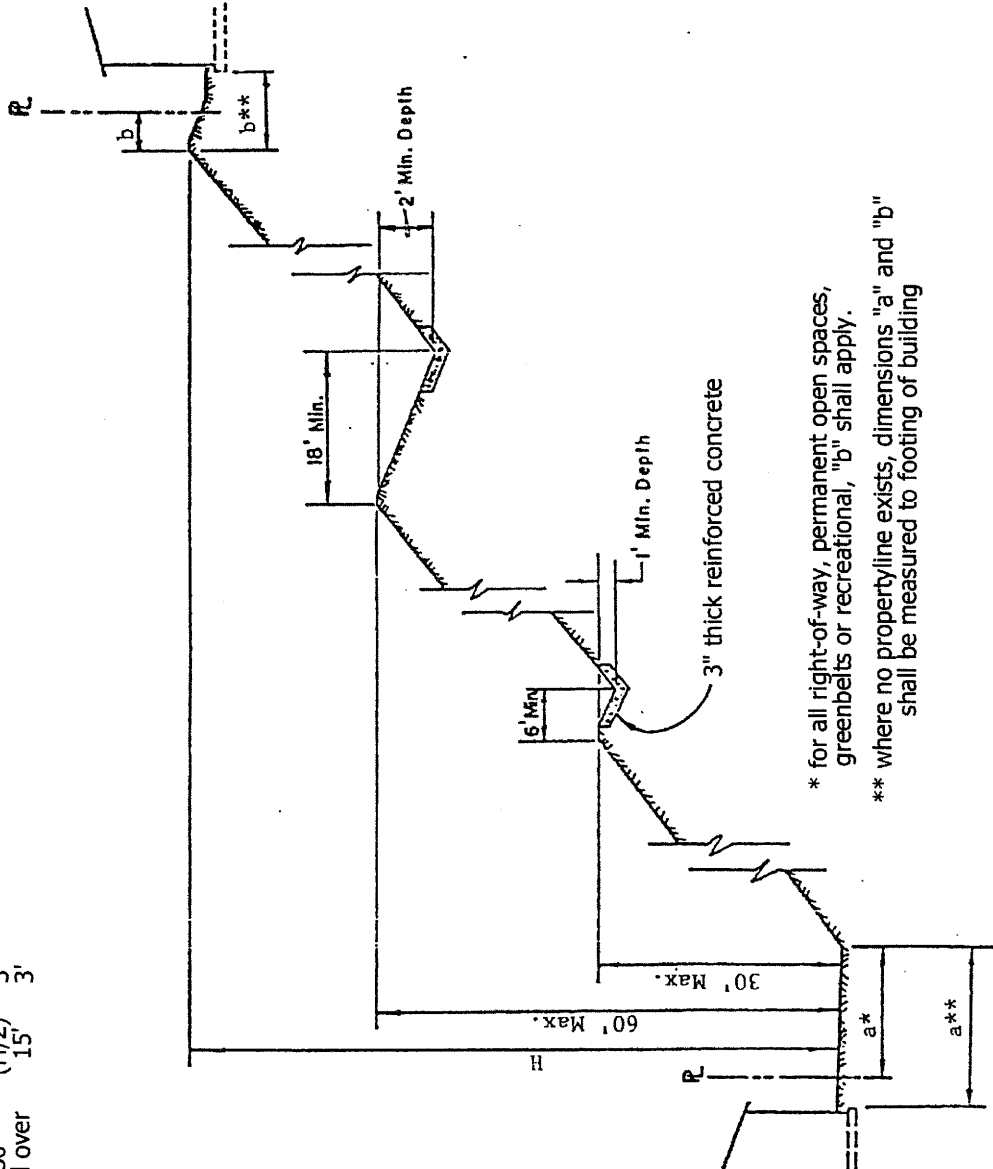


# APPENDIX B

## GRADING, EXCAVATIONS AND FILLS IN HILLSIDE AREAS

**TABLE NO. 17-A SETBACKS**

H in feet	a	b
0-10	3'	2'
11-30	(H/2)'	3'
31 and over	15'	3'



\* for all right-of-way, permanent open spaces, greenbelts or recreational, "b" shall apply.

\*\* where no propertyline exists, dimensions "a" and "b" shall be measured to footing of building

**APPENDIX C**  
**CITY OF ANAHEIM - CALIFORNIA**  
**COUNCIL POLICY**

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TRANSITIONAL AREAS

*It is the policy of the City Council that "Transitional Areas" be required between developed areas and "Hillside Grading." It is the intent that a transitional area be provided of a nature that will maintain and protect the desirable natural existing amenities of adjacent developed property. The scope and extent of the transitional area required will be determined by individual review by the Planning Commission and/or City Council.*

*The Council encourages that persons that propose to perform "Hillside Grading" participate in meaningful dialogue with adjacent citizens in developed areas to create a "spirit of understanding" and a shared concern that the environment will be protected and improved to the fullest extent practicable.*

CONTOUR GRADING

*It is the policy of the City Council to encourage contour grading to be performed by persons engaged in Hillside Grading. In keeping with needs to create an environmental community life that provides aesthetic scenic amenities that enhance human sensitivities by visually harmonious hillsides, contour grading shall be required except where determined to be impracticable by the City Engineer.*

HILLSIDE GRADING STANDARDS FOR  
CONTOUR GRADING

I. Policy Statements

1. *It shall be the policy of the City of Anaheim to encourage the use of imaginative grading techniques to improve the appearance of developments in hillside areas in general and in particular slope banks.*
2. *Linear slope banks, either cut or fill, are to be avoided.*

Revised:

# CITY OF ANAHEIM - CALIFORNIA

## COUNCIL POLICY

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3. *Cut and fill slopes in excess of 200 feet in length shall have curved linear configurations where consistent with the recommendation of the soil engineer, engineering geologist and City Engineer.*
4. *Where consistent with the recommendations of the soil engineer, engineering geologist and the City Engineer, a variety of slope ratios (for example: 1.75:1, 1.9:1, 2:1, 2.5:1, 3:1, etc.) shall be applied to any cut or fill slope in excess of 200 feet in length. The steepest permissible slope ratio shall be 1.5:1.*
5. *Where the slope configuration is controlled by the design of an arterial highway, slope banks in excess of 25 feet in height adjacent to and projecting above the roadway shall be screened by the use of planting bays at the toe of slope and adjacent to the road right of way. A variety of slope ratios and horizontal radii shall be used to blend into the adjoining slopes.*
6. *A variety of slope ratios and horizontal radii shall be used to blend manufactured slopes into the adjoining natural terrain in order to provide adequate transition and avoid abrupt changes between manufactured and natural slope banks.*

### II. Design Criteria

1. Curved Linear Slopes. *Where a cut or fill slope exceeds 200 feet in horizontal length, the toe and/or top of the slope shall be curved in a convex and concave manner to provide a variety of slope ratios. The radius at the toe of slope shall be no greater than 300 feet.*
2. Transition with Natural Slopes. *At the intersection of a manufactured cut or fill slope and a natural slope, a gradual transition or rounding or contours shall be provided with a minimum radius of 50 feet. Manufactured slope banks intersecting at or near right angles shall be rounded with a radius at the building pad of no less than 25 feet.*

Revised:

**CITY OF ANAHEIM - CALIFORNIA  
COUNCIL POLICY**

<b>Subject:</b>	<b>Policy Number</b>	<b>Effective Date</b>	<b>Page</b>
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3. Varying Slope Ratios. Where a manufactured cut or fill slope intersects with a natural slope, the manufactured slope approaching the natural slope shall utilize as nearly as possible the same ratio as the natural slope, but not steeper than 1.5:1. Where manufactured slope banks approach roadways at or near right angles above the elevation of the roadway, the slope shall be flattened at the point of intersection to a slope ratio of 3:1 or flatter. The corner of manufactured slope banks at street intersections shall be graded with a slope ratio of 3:1 or flatter through the curb returns.

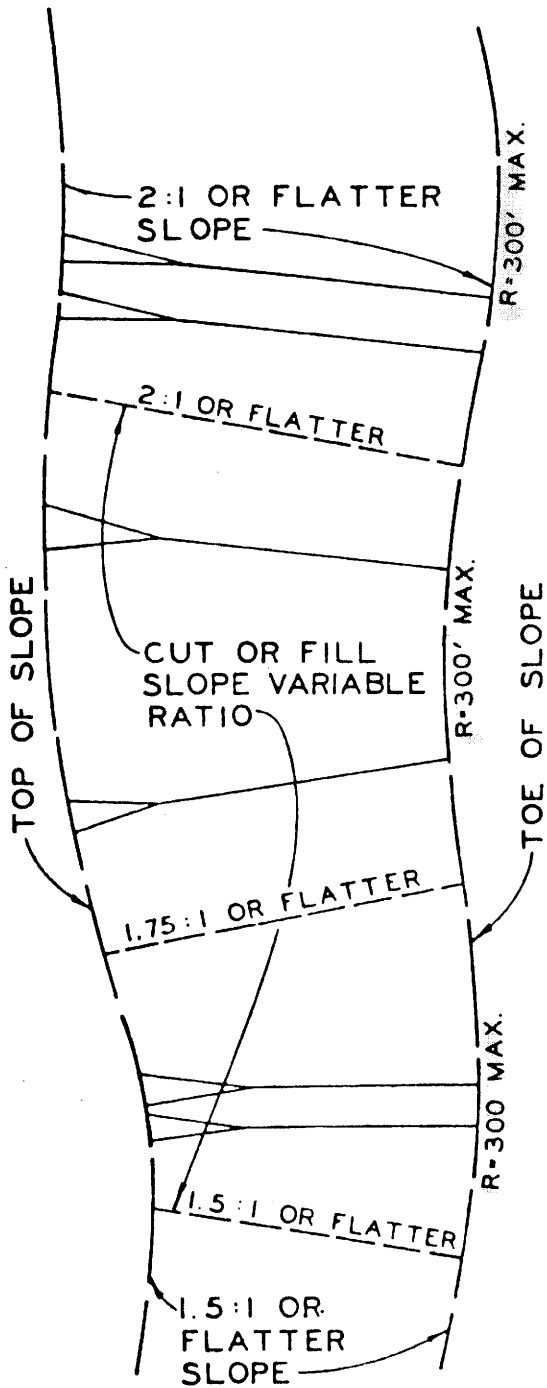
4. Planting Bays. Manufactured slope banks along arterial highways projecting above the roadway and exceeding 25 feet in height and 200 feet in length shall be screened through the construction of planting bays along the toe of the slope. Dimensions for these planting bays shall be as follows:

Slope Height:                    25 to 50 feet  
 Bay Width:                        10 feet  
 Minimum Length:                100 feet  
 Maximum Spacing:               250 feet between openings

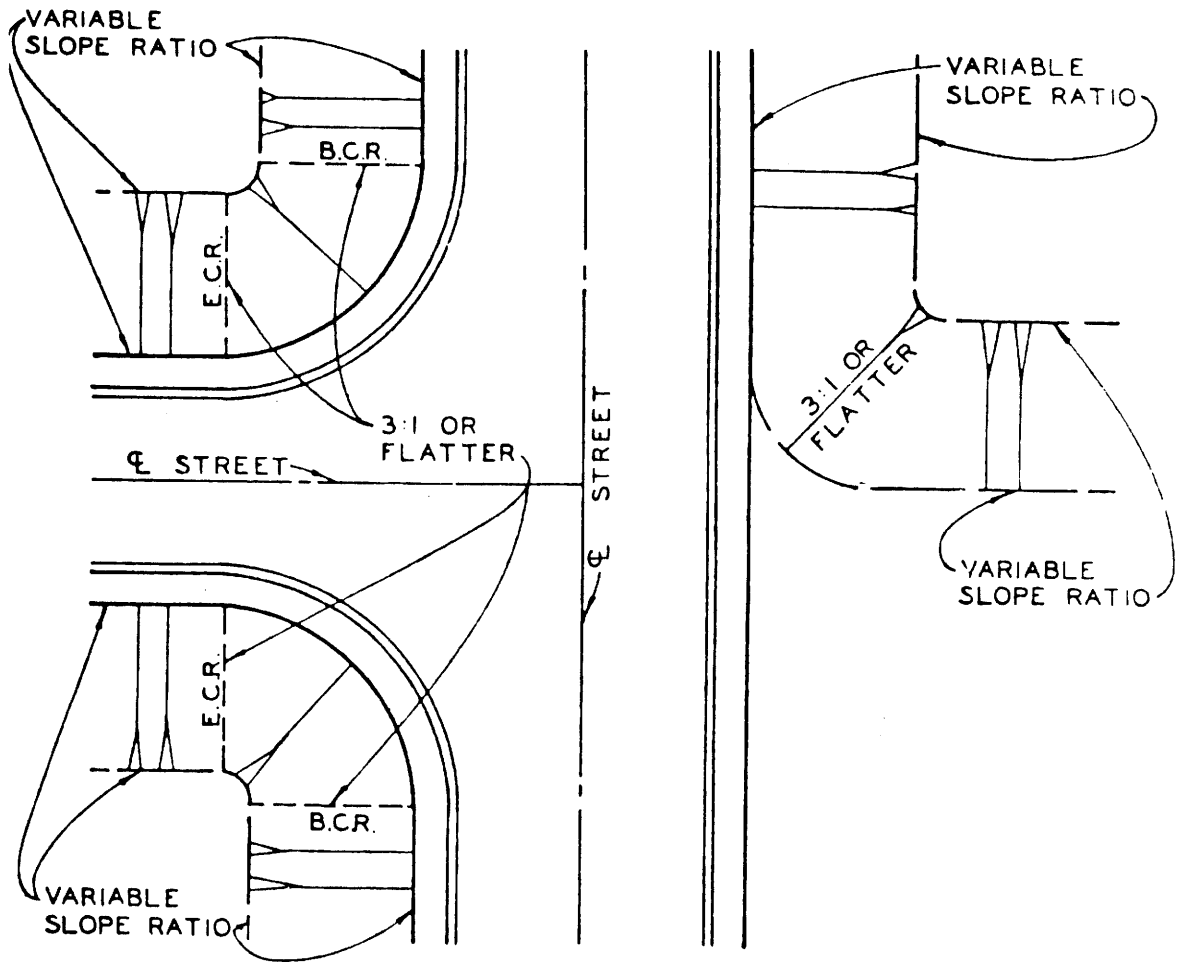
Slope Height:                    Over 50 feet  
 Bay Width:                        15 feet  
 Minimum Length:                100 feet  
 Maximum Spacing:               250 feet between openings

Planting bays shall further be contour graded and planted with specimen size trees. The sidewalk in these areas may be placed away from the curb line to meander through the landscaped bay area.

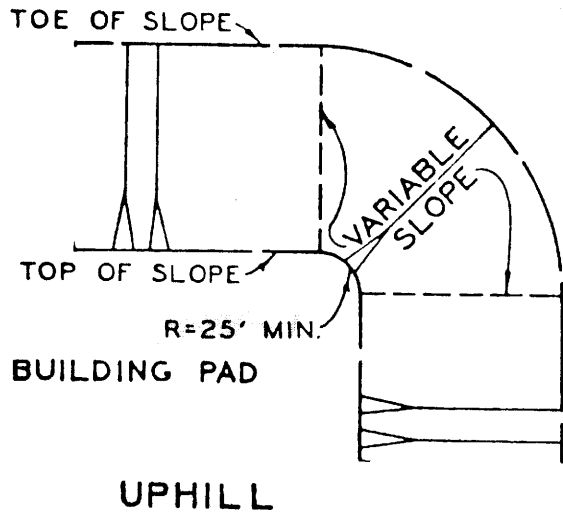
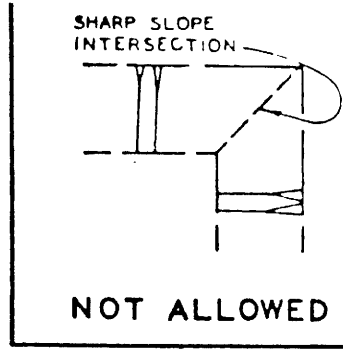
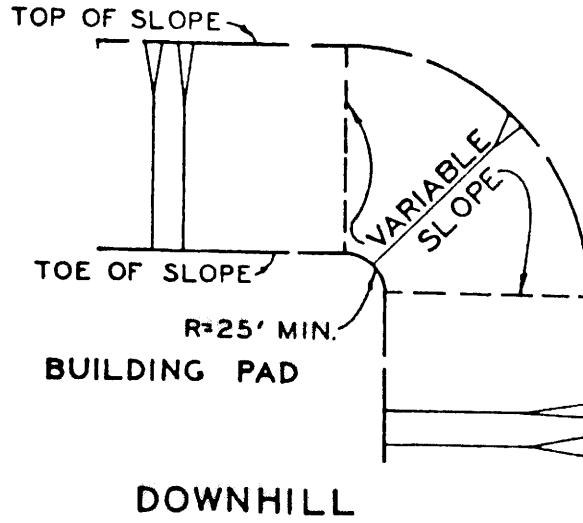
HILLSIDE GRADING

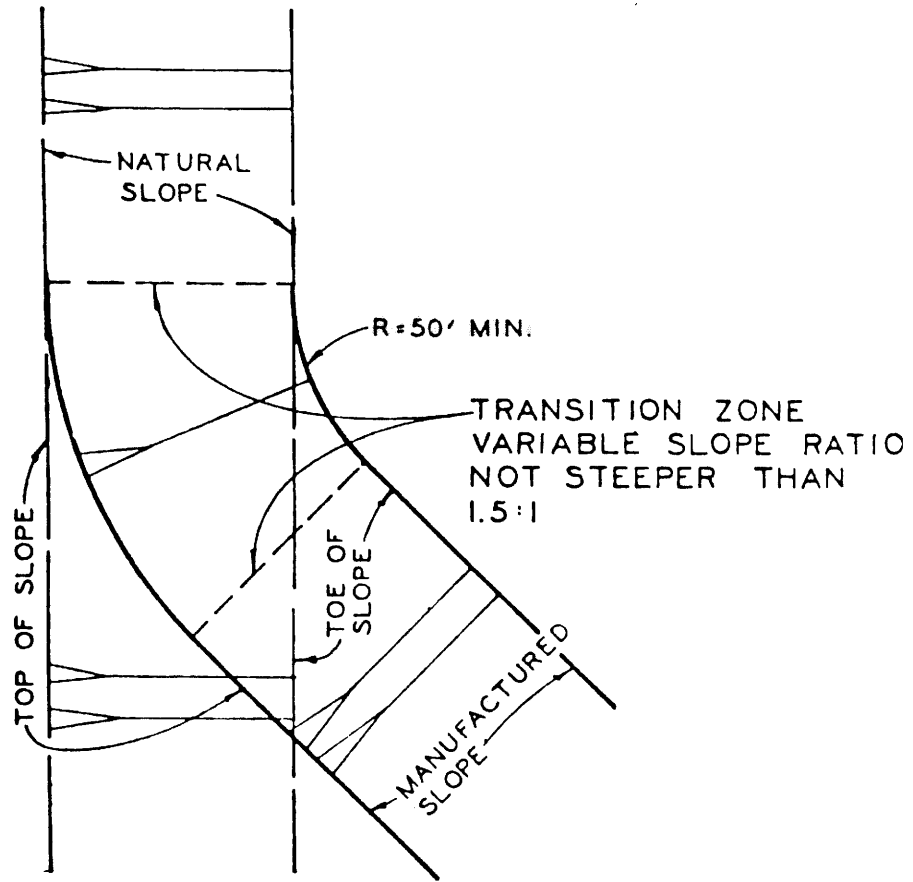


NOTE:  
SLOPE RATIOS SHALL  
VARY SUBJECT TO  
APPROVAL BY THE CITY  
ENGINEER.

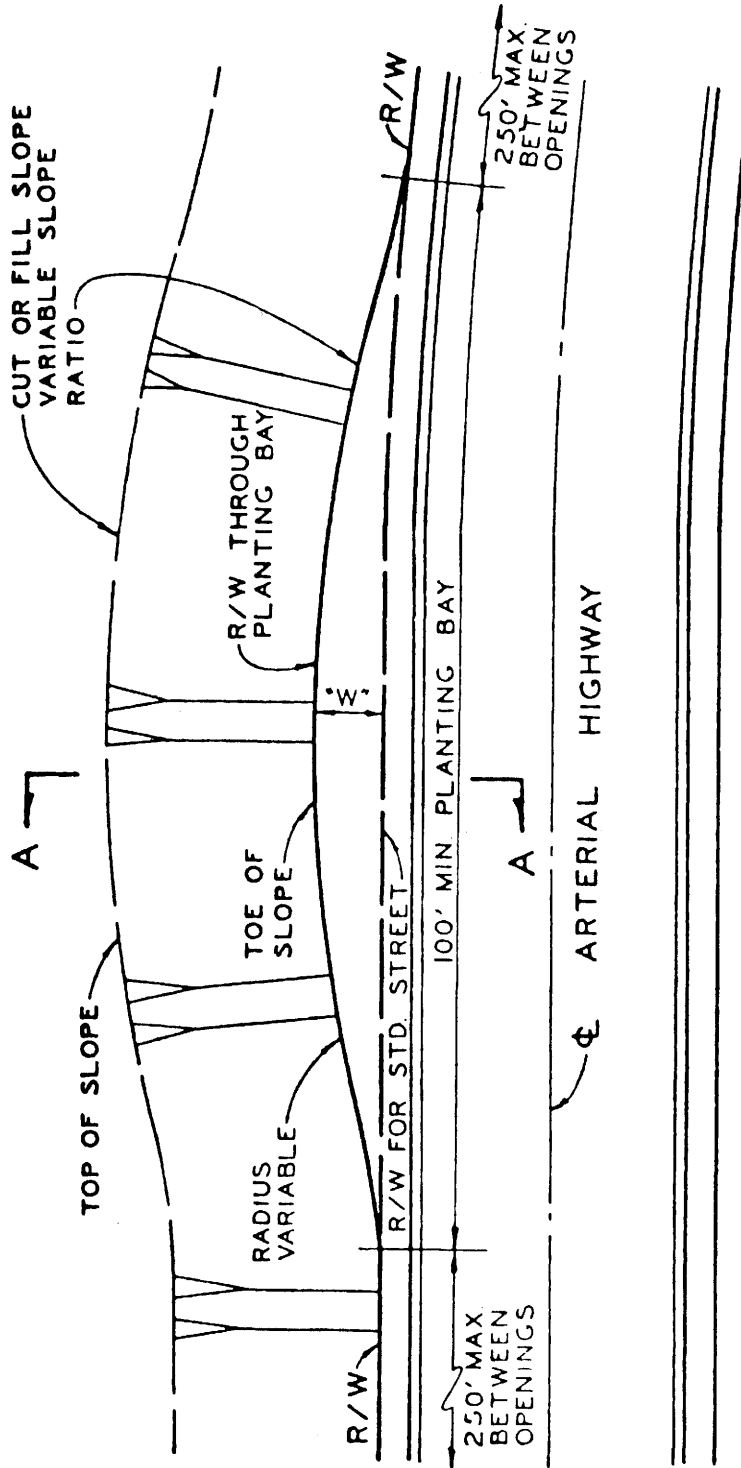


# HILLSIDE GRADING





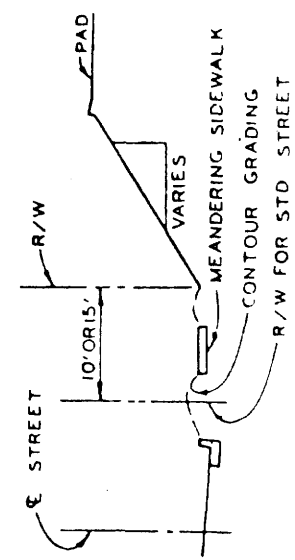




**CASE "A"**  
 SLOPE HEIGHT : FROM 25' TO 50'  
 W = 10' MIN.

**CASE "B"**  
 SLOPE HEIGHT : FROM 50' AND ABOVE  
 W = 15' MIN.

MIN. PLANTING  
 CASE "A" 3 TREES ~ MIN 10' HIGH  
 CASE "B" 3 TREES ~ MIN 15' HIGH



SECTION A - A

REF: HILLSIDE GRADING ORD. SEC. 17.06.132